

5C SERIES



DEUTZ-FAHR
5120 C





THE 5C SERIES.
THE COMFORT CLASS OF
THE UTILITY CLASS.



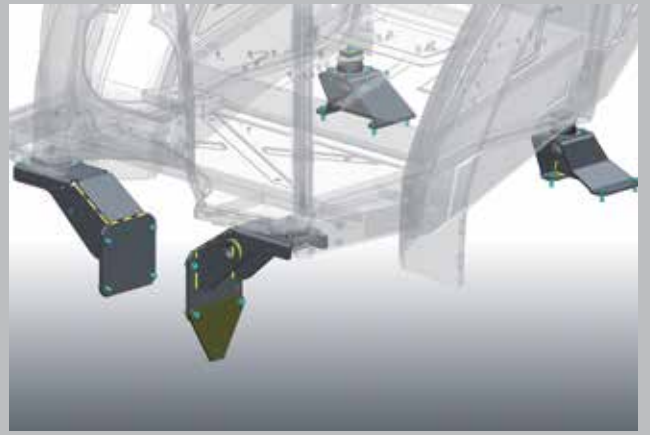
5C SERIES	5120
Max. power (kW/HP)	87/118

For efficient work with utility tractors, working, driving and operating comfort are just as important as versatility. Arable farms, dairy farms, livestock farms and municipal applications – the range of work is increasingly varied, and the working day increasingly long. Only if a utility tractor meets every aspect of the working profile can it be commercially successful. Only if the level of comfort is right will the performance and motivation of drivers remain high for a long time. DEUTZ-FAHR has developed the new 5C Series with this in mind. The highly comfortable cab of the new 5C Series is supported on the latest design of "Hydro Silent-Block" cabin suspension mounts. The gear shift lever has been redesigned as the gears are now easily

selected using a Bowden cable design. An up to 40% reduction in cab noise and vibrations is the result. At the same time, the new light package guarantees optimal vision during poor visibility. Whatever demands you have of a utility tractor, DEUTZ-FAHR can meet them with state-of-the-art technology, a wide range of configuration options and an extraordinary level of comfort. At DEUTZ-FAHR you will find the largest selection of utility tractor models.



THE CAB. WORKING COMFORT
WITH CUTTING-EDGE
TECHNOLOGY.



Cab vibrations reduced up to 40% thanks to "Hydro Silent-Block" suspension.



The lights package. For optimal visibility when visibility is poor. Eight working headlights in the roof and side posts. Two working headlights and two LED lights in the bonnet.



Optimal visibility upwards.

Anyone who sits inside the new 5C Series will immediately notice that the new cab is the embodiment of comfort and peace. The design is similar to that of a comfortable car. Smooth curves, a bright, friendly atmosphere, controls within easy reach and clear visibility on all sides promote fast and efficient work. As in a comfortable car when you start the engine, you feel pure power and nothing else. Noise and vibrations remain outside. The cab is supported on the latest design of "Hydro Silent-Block" cabin suspension mounts that absorbs up to 40% more vibrations than the previous versions. At night or when visibility is poor

due to adverse conditions, the light package with eight work lights (six in the roof and two on the side posts) plus two working headlights and two LED lights in the bonnet guarantees perfect illumination of the entire working area. Whatever tractor tasks need to be carried out, the modern and stylish cab motivates drivers to enjoy their daily workplace. Day after day.



DEUTZ-FAHR CONVENIENCE
MEANS THAT EVERYTHING WORKS
AT THE PUSH OF A BUTTON.



For DEUTZ-FAHR, working comfort means that the tractor adapts to the driver and not vice versa. The cab has been revised with the aim of making the operation fast, straightforward and intuitive for all kinds of work. As is customary with DEUTZ-FAHR, all controls are clearly colour-coded and logically arranged according to their frequency of use. The illuminated InfoCentre in the steering column provides the driver with with all the required information about the tractor's operating status – logical and clearly presented. The re-designed gear shift lever now allows convenient gear shifting at the push of a button using Bowden cables. In another new development, the auxiliary valve controls are operated in the right-hand console. These can be used in a wide range of operating modes and locking functions, which can be selected precisely, conveniently and reliably. Furthermore, it is now possible to store an engine speed. During routine work, the most efficient range for working or driving can be activated at the push of a button.



Bowden cables have made gear changing even more convenient at the push of a button.



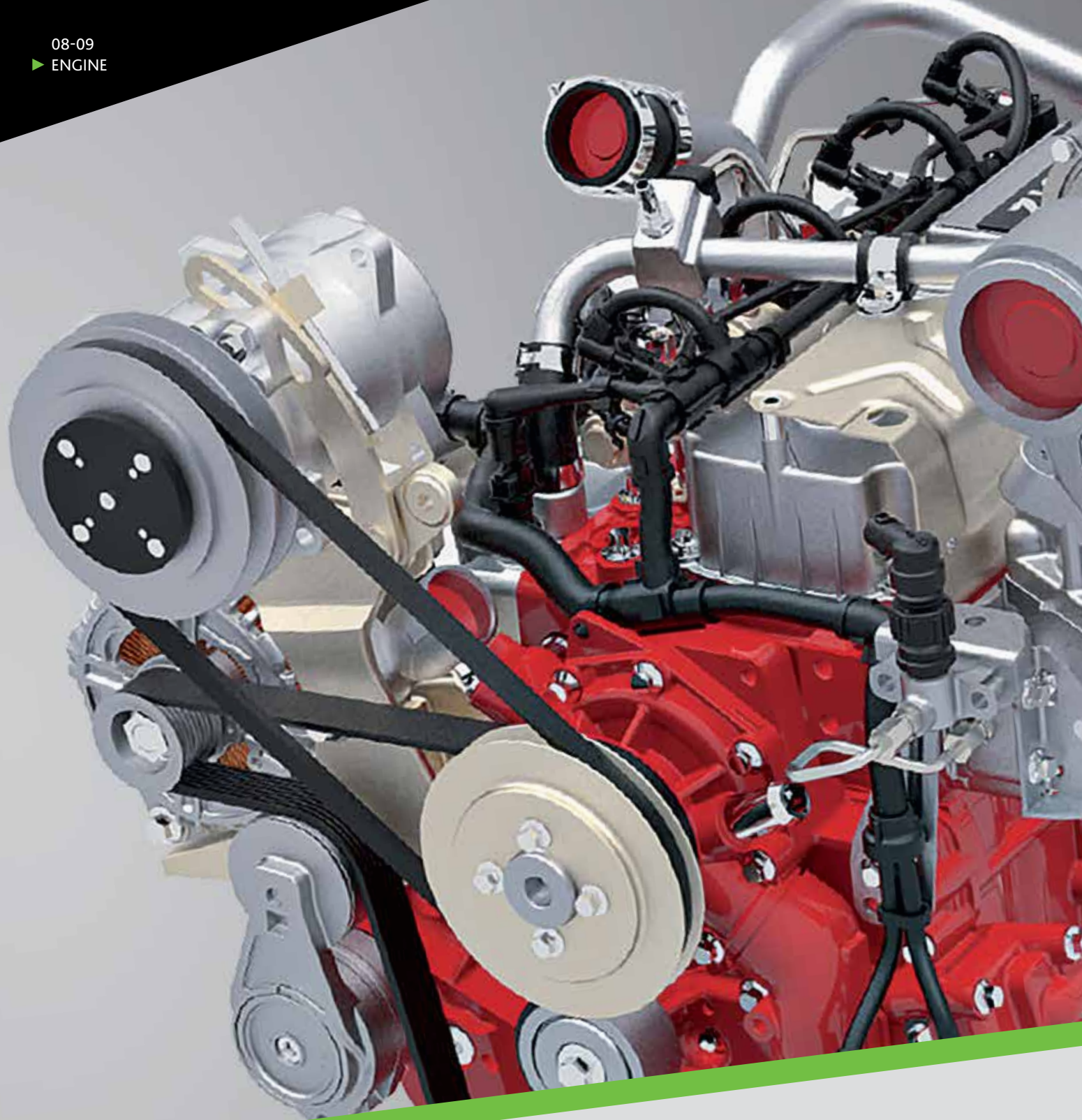
New auxiliary valve control operation. Precise, highly convenient and extremely reliable for all functions.



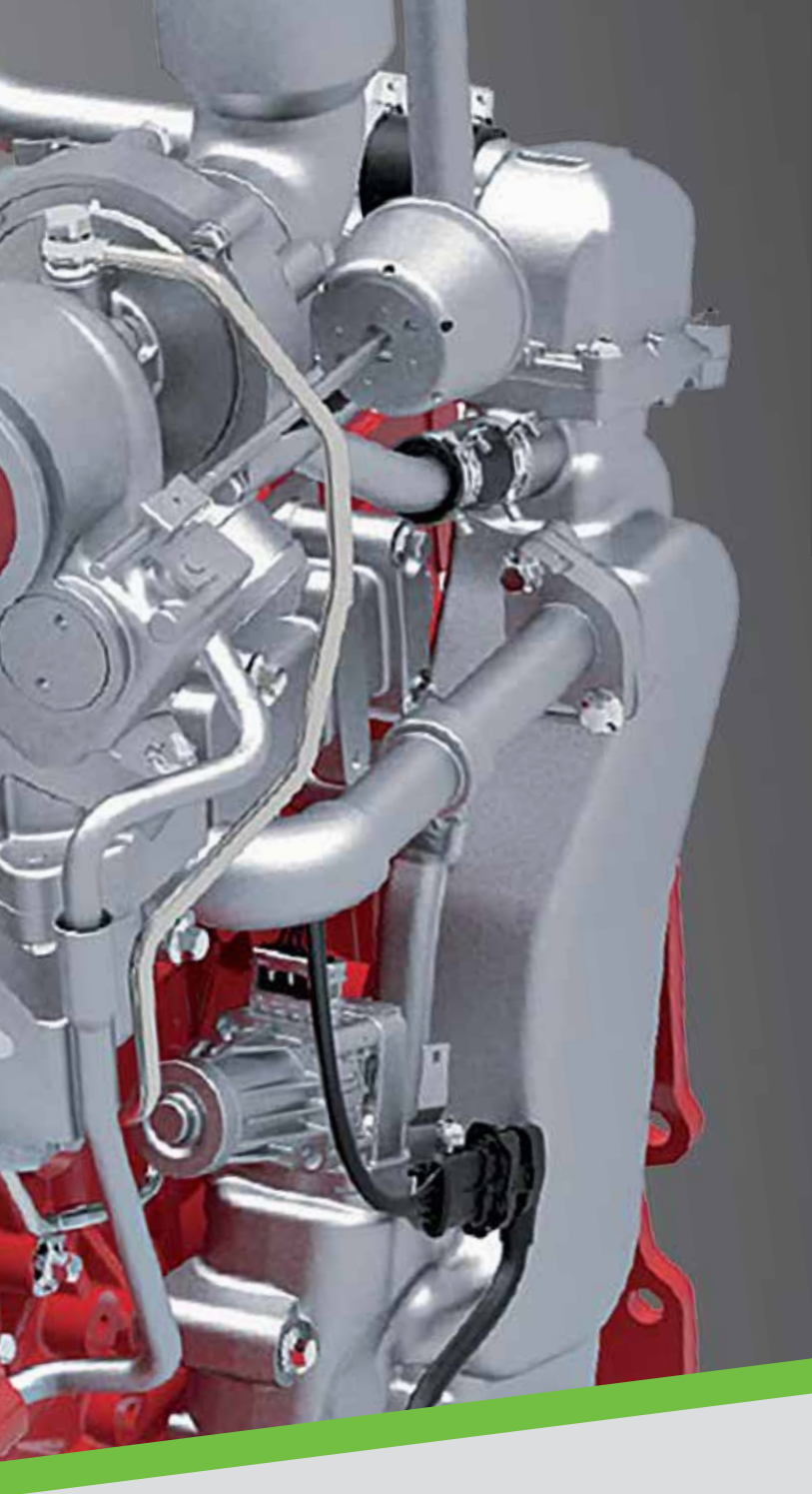
The InfoCentre. Every operating status constantly in view. Analogue and digital.



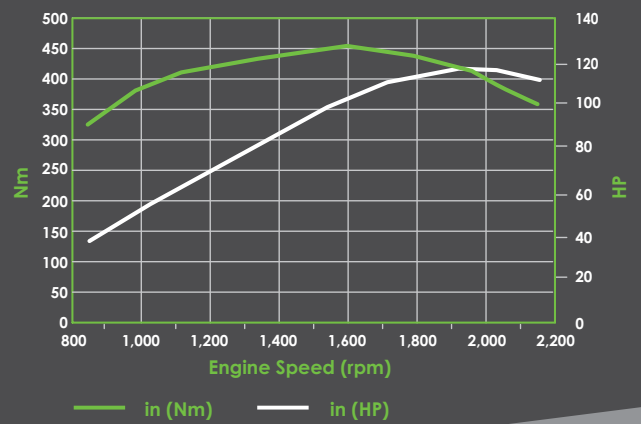
Engine speed settings. Pre-programmed efficiency for routine operations.



DEUTZ 3.6 COMMON RAIL.
BIG PERFORMANCE.
LOW FUEL CONSUMPTION.



Power and torque curves for 120 model.



Diesel oxidation catalyst (DOC) catalytic converter guarantee minimum emissions.



The new PowerCore air filter supports engine efficiency ...



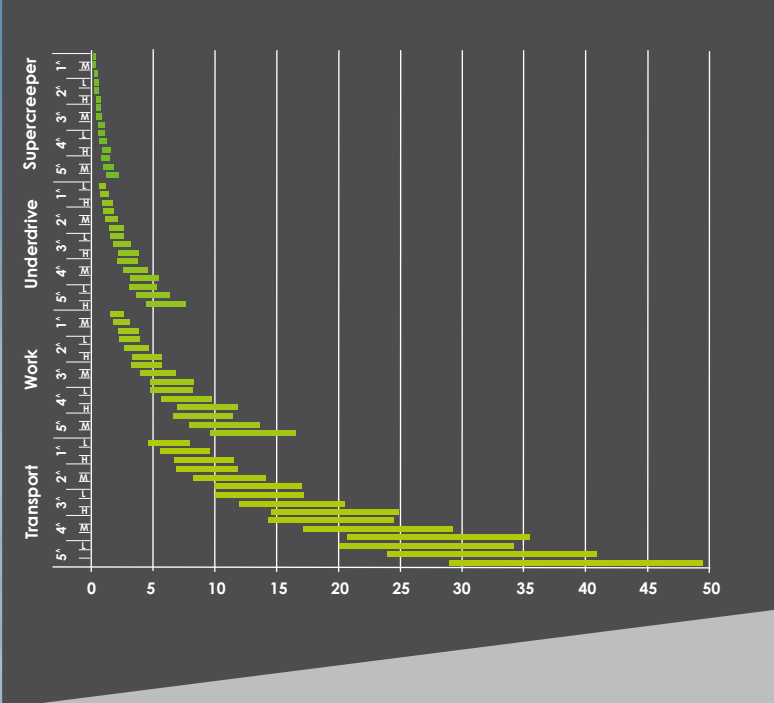
... as does the easy-to-clean cooling system with its viscous fan (optional).

The new engine powering the 5C Series is the Deutz TCD 3.6 L04, a 3,620 cm³ four-cylinder unit bristling with state of the art technology which delivers extraordinary performance, reliability and durability. The innovative technology employed include electronically controlled Common Rail injection, a turbocharger with intercooler and wastegate valve, and a proportional viscstatic fan. This impressive package of technical solutions has made it possible to keep specific fuel consumption down to a record-breaking 210 g/kWh. Compliance with Tier 4i emissions regulations is ensured by an electronically controlled, liquid cooled external exhaust gas recirculation system (EGR) and a diesel oxidation catalyst (DOC). The DOC (Diesel Oxidation Catalyst) installed above the engine catalyses the components of the exhaust gas on contact to transform them into substances that are harmless for the environment. As it does not function as a particulate filter, it does not produce wasteful exhaust back-pressure and does not require energy-costly active regeneration cycles. It needs no additives, such as AdBlue used in selective catalytic reduction systems (SCR), and is maintenance-free. It

is also less bulky than an SCR system, making it better suited for installation on compact tractors. With a fully electronic engine governor, which means that the engine delivers only the power actually necessary to perform the specific, and a 150 litre fuel tank, these tractors have the autonomy needed to work continuously throughout even the longest day. These engines are designed specifically for agricultural applications, as demonstrated by a torque curve which remains close to maximum torque from 1,200 to 1,900 rpm - the engine speed range used in the majority of agricultural applications - and a torque reserve of 32%. The power curve, on the other hand, is practically flat between 1,800 and 2,200 rpm, ensuring silky smooth operation even when performing particularly heavy-duty tasks.



EFFICIENT TRANSMISSION.
SELECT THE GEAR WITHOUT
COMPROMISE.



Comfort clutch and Powershift. Simply change gear at the push of a button without pressing the clutch pedal.



Powershuttle as standard. Electro-hydraulic reverse shuttle with adjustable five stage settings



Stop and Go activation via push button.

When it comes to the transmission, the new 5C Series shows just how adaptable a utility tractor can be. For all variants, maximum efficiency is guaranteed by a highly efficient five-speed transmission with electro-hydraulic reverse shuttle and up to four gear ranges (slow, fast, creep and super creep) with three Powershift speeds and 60 forward and reverse gears. Driving at 40 km/h ECO with a reduced engine speed goes without saying. Thanks to Powershift technology, engine speed can be increased or decreased under load simply by pressing a button on the gear shift lever without activating the clutch. The electro-hydraulic reverse shuttle is equipped with two long-life wet multi-plate clutches. This makes it possible to change direction at speeds up to 13 km/h even under load. Standard equipment includes Powershuttle and the convenient Stop&Go function for stopping and moving off without using the clutch.



FRONT AXLE AND BRAKES.
MAXIMUM DRIVING COMFORT
AND SAFETY BOTH IN THE FIELD
AND ON THE ROAD.



A unique safety concept from DEUTZ-FAHR. Separate oil cooled disc brakes on the front wheels.



Mechanical parking brake.



The SDD rapid steering system. Half the wheel rotation, double the steering power.

The front axle concept of the new 5C Series guarantees driving comfort, manoeuvrability, safety and traction in all ground conditions. The compact bonnet allows a steering lock angle of 55 degrees. The SDD rapid steering system, which halves the number of steering wheel turns needed, is available as standard. Safety is a top priority for DEUTZ-FAHR. The front axle – just like the rear axle – is fitted with disc brakes.

These engage without engagement of the front drive. Electro-hydraulic engagement of the differential lock and four-wheel drive is also standard. All models have a 100% locking of the front and rear axle.



STRENGTH AND PRECISION.
CONTROLS AT YOUR FINGERTIPS
FOR SIMPLE, PRECISE OPERATION.



New lift control buttons.



Control panel for rear lift settings.



External PTO and rear lift controls for straightforward equipment mounting and external PTO operation.

The electronically controlled (EHR) standard rear lift uses a pair of extension sensing load cells situated near the lower link arms as draft sensors. Maximum lifting capacity, measured at the quick hitches for the 5120 C is an impressive 5410kg (in standard configuration). Boasting extraordinarily precise draft and position control, in addition to mixed mode (and, of course, floating mode), the EHR electronic lift management system also performs a number of other functions: quick soil engagement, oscillation damping for transport use with implements

mounted on the 3-point linkage, position lock, lift height limiter and rate of drop control. Controlling the lift from the cab is made even simpler by the new control button layout and an additional control in a set back position to the right of the driver seat for easily adjusting the position of the lower arms of the rear lift when hitching implements. Maximum load capacity of the (optional) front lift is 1,850 Kg at the quick hitches.



MAXIMUM PRODUCTIVITY
ALWAYS.



Front and rear PTO activation buttons.



PTO speed selector lever with the selection of 4 speeds.

Being able to choose from many different PTO speed alternatives contributes significantly to the versatility of the tractor, and makes it possible to maximise efficiency even in applications without high power demands. All four of the standardised PTO speed modes (540, 540ECO, 1,000 and 1,000 ECO) are available with the 5C Series. All speeds are electrohydraulically engaged with an automatic load detection function, to ensure smooth, progressive implement starting and take full advantage of the extraordinary performance of the Deutz engine in all conditions. The PTO

controls are simple and rational: with electrohydraulic controls and an LCD screen displaying the effective PTO speed, allowing the driver to use any implement in the ideal operating conditions. The PTO engage button is duplicated on the rear mudguards, while an automatic function, which engages and disengages the PTO in relation to the position of the implement hitched to the rear 3-point linkage, is also available. Tractors configured with the front lift may also be equipped with an optional front PTO which operates at a standard 1,000 rpm.



5C SERIES.
COMFORTABLY IN FRONT
WITH FRONT END LOADER WORK.



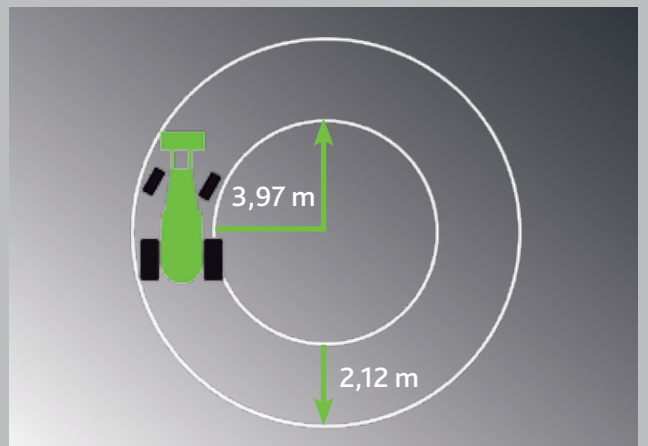
Feed, grain, soil, bales or even working platforms – there is always something to lift, shovel, load or move around each day. Just one look at 5120C is enough to know that these are the ideal front end loader tractors. With adjustable Powershuttle, Stop&Go, and SDD rapid steering systems, they are versatile and easy to manoeuvre. The additional disc brakes on the front wheels ensure safety. The sunroof offers an optimal view of the entire working area. The front end loader joystick is conveniently operated by a fully integrated lever ergonomically positioned for full control.



Push buttons to activate the Stop&Go and SDD functions.



The loader control joystick integrates perfectly in the right-hand console.



A turning circle of 3.97 m allows easy manoeuvring even in the tightest of spaces (depending on tyres and surroundings).



Wide glass sunroof for optimal vision when using front end loader.



LOW OPERATING COSTS.
SIMPLIFIED MAINTENANCE WITH
EASY ACCESS TO COMPONENTS.



Oil check.



Radiators.



Air filter.

On the 5C Series, all the major components and systems are easily accessible, and all routine checks may be performed without tools. The engine oil level can be checked without even opening the cowl, while the transmission/hydraulic oil is checked from a dedicated sight glass. The cooling system also boasts trouble-free maintenance, with pull-out radiators that slide out laterally and an easily removable anti-fouling screen. Both the engine air and cab air filters are easy and quick to service. All fuses and relays are contained in a specific compartment

in the right hand mudguard, where there are easily accessible but well protected. A battery master switch disconnects the battery and allows the tractor to be safely put in storage for extended periods. And for even greater reliability, the air compressor (if installed) is mounted on the engine and driven directly without an additional belt. Engine oil changes are only necessary every 500 operating hours.

MODEL 5120 C

ENGINE

DEUTZ TCD 3_06 L04 Tier 4i Diesel Engine
 4 Cyl/3620cc Turbocharged/Intercooled
 Electronically Controlled
 Emission control by DOC unit
 Liquid/Oil Cooled with Visco fan
 High pressure Diesel injection/common rail
 118hp Max hp @ 2200 rpm (2000/25/CE)
 5 Hp Power Boost
 Torque 460Nm @1600 rpm
 100% Bio Diesel Compliant
 Programmable Engine Speed
 Powercore Air Filter with Dust Ejectors
 Side vertical exhaust
 Fuel Tank 150 litres

TRANSMISSION

60/60 forward/reverse gears
 Wet clutch Powershuttle
 Shuttle modulation with 5 settings
 5 speeds/4 Ranges / 3 stage Powershift
 Comfort Clutch
 Stop and Go function
 Front Wheel Assist – Push Button
 4 Wheel Independent Braking
 100% Front & Rear Diff Lock - Push Button
 40 km/h Road speed @ Super Economy RPM

PTO

Independent wet multidisc clutch
 4 Speed PTO 540/540E- 1000/1000E
 Push Button Control in cab/rear fender
 Auto PTO
 Interchangeable PTO Spline Function
 1" 3/8 – 6 Spline PTO shaft

WHEEL EQUIPMENT & MISCELLANEOUS

Fixed Fully Welded Radial Wheel Equipment
 440/65R28 Front -540/65R38 Rear Tyres
 Front Weight Frame

OPERATOR ENVIRONMENT

D2L Class Air Conditioned Cab
 Hydro Silent Block Cabin Suspension
 Super Deluxe Air Seat
 Tilt and telescopic steering wheel
 SDD Rapid Steering circuit
 Hi Visibility Glass Roof/rear wiper
 INFOCENTER Digital Display
 MP3 Radio Player with Twin Speakers
 Telescopic Mirrors
 6 Cabin Work lights
 2 Bonnet Front Work lights
 2 Work lights on Cab Post
 Rotating Beacon

HYD/LINKAGE

Open Centre Hyd Pump System
 55 L/min Hyd flow at Rear Remotes
 Separate 40L/min Steering Pump
 3 Sets of Remotes with Float
 Electronic hitch with position & draft control
 Free flow return Line
 External Raise & lower on Rear Fenders
 Cat 2 Hook ends--telescopic stabilisers
 5410 kg lift capacity with Assistor Rams
 12v elec. socket and 7-pin ISO 11786 plug

WEIGHTS & DIMENSIONS

Wheelbase 2400 mm
 Length 4314mm
 Total Height 2730mm
 Total Weight 4200kg



Dealer

Produced by Marketing/Communication Department - Code 308.83513.4-4 - 05/14 I.P.

